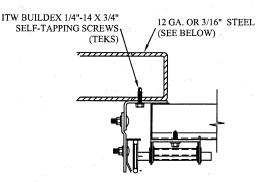


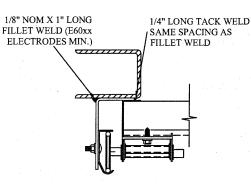
TRACK CONNECTION DIRECTLY TO STRUCTURE OPTIONS



CLIP STYLE REVERSE ANGLE MOUNT SHOWN BRACKET, CONTINUOUS AND TAPERED ANGLE MOUNT AVAILABLE

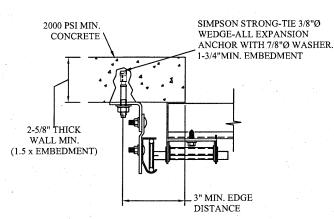
12 GA. STEEL FRAMING
232 LBS./SCREW ALLOWABLE LOAD - 6" FROM ENDS
AND 12" O.C.
REFER TO NOTES: 1, 2 AND 5

3/16" STEEL FRAMING 569 LBS./SCREW ALLOWABLE LOAD - 6" FROM ENDS AND 24" O.C. REFER TO NOTES: 1, 2 AND 5



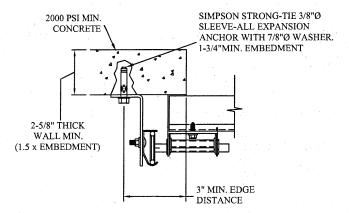
REVERSE ANGLE MOUNT SHOWN
BRACKET, CONTINUOUS AND TAPERED
ANGLE MOUNT AVAILABLE

STEEL FRAMING 12GA OR BETTER 1590 LBS./IN. ALLOWABLE LOAD - 6" FROM ENDS AND 24" O.C. REFER TO NOTES: 1, 2, 5, 6, 7, 8 AND 9



CLIP STYLE CONTINUOUS ANGLE MOUNT SHOWN BRACKET, REVERSE AND TAPERED ANGLE MOUNT AVAILABLE

2000 PSI CONCRETE OR GREATER
351 LBS./EXPANSION ANCHOR ALLOWABLE LOAD - 6"
FROM ENDS AND 18" O.C.
REFER TO NOTES: 1, 2, 3, 4 AND 5



CONTINUOUS ANGLE MOUNT SHOWN BRACKET, CONTINUOUS AND TAPERED ANGLE MOUNT AVAILABLE

2000 PSI CONCRETE OR GREATER
336 LBS./EXPANSION ANCHOR ALLOWABLE LOAD - 6"
FROM ENDS AND 18" O.C.
REFER TO NOTES: 1, 2, 3, 4 AND 5

SPECIFICATIONS AND NOTES

1. ALL THE LOAD FROM THE DOOR IS TRANSFERRED TO THE VERTICAL TRACK, FROM THE TRACK THE LOAD IS TRANSFERRED TO THE VERTICAL JAMBS. THE HORIZONTAL JAMB OR HEADER RECEIVES NO PORTION OF THE LOAD TRANSFERRED FROM THE DOOR.

2. EACH VERTICAL JAMBS RECEIVES MAXIMUM DESIGN LOADS OF: +223.1 LBS/FT & -254.3 LBS/FT

3. DOOR AND HARDWARE WILL BE DESIGNED, MANUFACTURED AND INSTALLED WITH STANDARDS AS SET FORTH BY DASMA.

4. DOOR SECTIONS SHALL BE $\,$ 24 GA. (.021) MIN. EXTERIOR SKIN ROLLED FORMED, W/ BAKED ON POLYESTER FINISH

5. DOORS UP TO 24'0" HIGH USE (2) 3" 20GA STRUTS ON EVERY OTHER SECTION STARTING ON THE BOTTOM SECTION & (1) 3" 20GA STRUT ON EVERY OTHER SECTION STARTING ON WITH THE SECOND SECTION.

6. SUPPORTING STRUCTURAL ELEMENTS SHALL BE DESIGNED BY A REGISTRED PROFESSIONAL ENGINEER FOR WIND LOADS INDICATED ON THIS DRAWING IN ADDITION TO OTHER LOADINGS.

WOOD JAMB ATTACHMENT TO STRUCTURE (OPTIONAL)

2 X 6 VERTICAL JAMB ATTACHMENT TO WOOD FRAME STRUCTURE 5/16" X 3" LAG SCREWS STARTING 6" FROM ENDS

THEN 22" O.C. (1 1/2" EMBEDMENT)

2 X 6 VERTICAL JAMB ATTACHMENT TO 2,000 PSI CONCRETE HILTI KWIK BOLT 3/8" X 4" STARTING 6" FROM ENDS

THEN 24" O.C. (2 1/2" EMBEDMENT) HILTI SLEEVE ANCHOR 3/8" X 2-3/4" STARTING 6" FROM ENDS THEN 18" O.C. (1 1/4" EMBEDMENT)

ITW/RAMSET REDHEAD (TRU-BOLT) 3/8" X 4" STARTING 6" FROM ENDS THEN 24" O.C. (2 1/2" EMBEDMENT)

2 X 6 VERTICAL JAMB ATTACHMENT TO HOLLOW C-90 BLOCK SIMPSON 1/4" X 3" TITEN SCREWS STARTING 6" FROM ENDS.

USE PAIRS OF FASTENERS (3" APART)

AT 16" O.C. (1 1/2" EMBEDMENT)

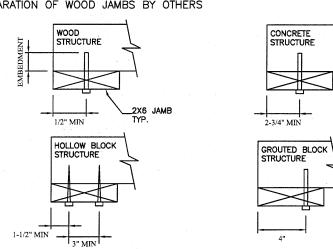
HILTI 1/4" X 2-3/4" KWIK-CON II+ SCREWS STARTING 6" FROM ENDS, USE PAIRS OF FASTENERS (3" APART) AT 16" O.C. (1 1/4" EMBEDMENT)

2 X 6 VERTICAL JAMB ATTACHMENT TO GROUTED C-90 BLOCK (2000 PSI GROUT)

HILTI SLEEVE ANCHOR 3/8" X 2-3/4" STARTING 6" FROM ENDS THEN 22" O.C. (1 1/4" EMBEDMENT)

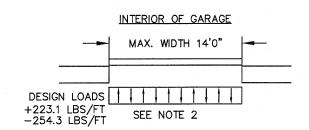
(OR, USE FASTENERS FOR HOLLOW C-90 BLOCK)

*LAGS AND BOLTS CAN BE COUNTERSUNK TO PROVIDE A FLUSH MOUNTING SURFACE. *PREPARATION OF WOOD JAMBS BY OTHERS



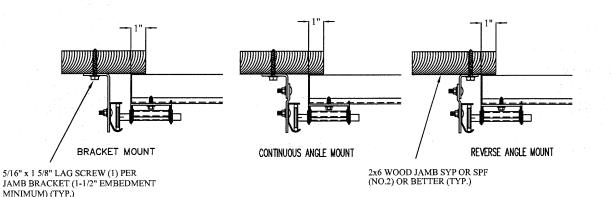
NOTES:

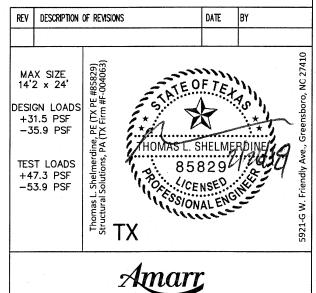
- 1. ANCHORS TO BE EVENLY SPACED BETWEEN THE HEADER AND FLOOR.
- 2. FIRST (BOTTOM) ANCHOR STARTING AT NO MORE THAN HALF OF THE MAXIMUM ON-CENTER DISTANCE. HIGHEST ANCHOR INSTALLED AT LEAST AS HIGH AS THE DOOR OPENING.
- 3. MIN. EGDE DISTANCE OF 3" REQUIRED.
- 4. USE WASHERS PROVIDED BY THE ANCHOR MANUFACTURER.
- 5. SUPPORTING STRUCTURAL ELEMENTS SHALL BE DESIGNED BY A REGISTERED PROFESSIONAL ENGINEER FOR WIND LOADS IN ADDITION TO OTHER LOADS.
- 6. MOST GARAGE DOOR TRACK IS GALVANIZED STEEL. USE ALL NECESSARY PRECAUTIONS WHEN WELDING GALVANIZED STEEL.
- 7. ALL WELDS SHOULD BE PERFORMED BY A CERTIFIED WELDER OR INSPECTED BY A CERTIFIED WELDING INSPECTOR TO VERIFY THE INTEGRITY OF THE WELD.
- \$, FILLET WELDS TO HAVE A STRAIGHT OR CONVEX FACE SURFACE.
- 9. TACK WELD TOE OF ANGLE AT SAME SPACING TO PREVENT ROTATION OF TRACK ANGLE.

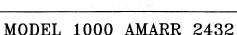


TRACK CONNECTION TO WOOD JAMB OPTIONS

FOR LAG SCREWS & BRACKET SPACING SEE PAGE 4 FOR TRACK CONFIGURATION DETAIL

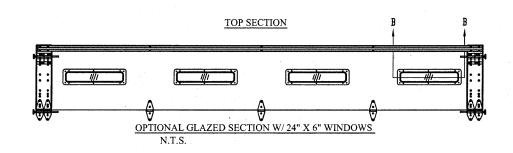


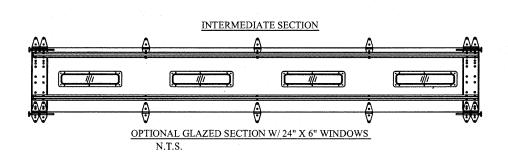


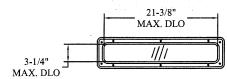


SIZE	DRAWN BY	RLR	DATE	6/6/17	DRAWING NUMBER					
В	CHECKED BY	RLR	DATE	6/6/17	IBC-1014-150-15					
165 CA	PRIAGE COLL	SHEET 2 OF 4								

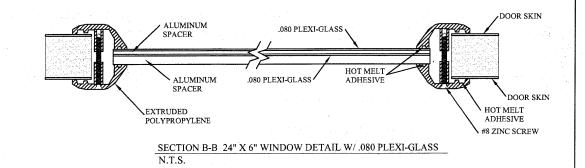
ENTRE MATIC

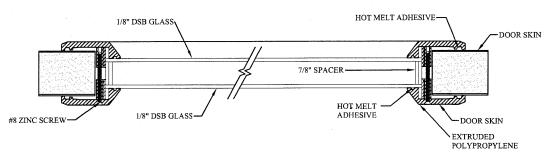




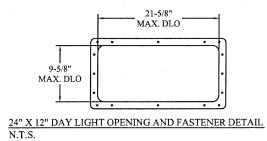


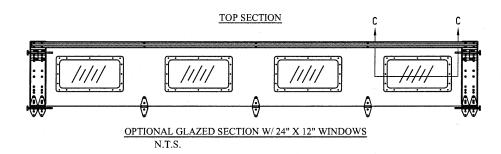
 $\frac{24"~X~6"~DAY~LIGHT~OPENING~AND~FASTENER~DETAIL}{\rm N.T.S.}$

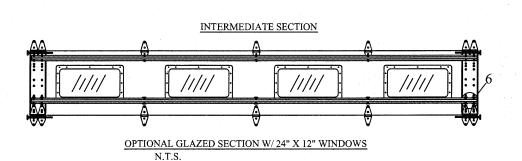


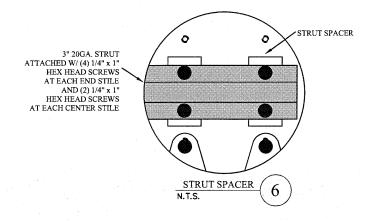


SECTION C-C 24" X 12" WINDOW DETAIL W/ 1-1/8" INSULATED GLASS N.T.S.









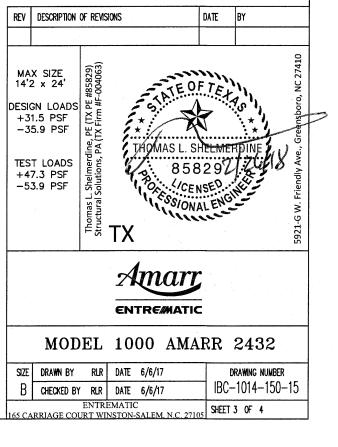
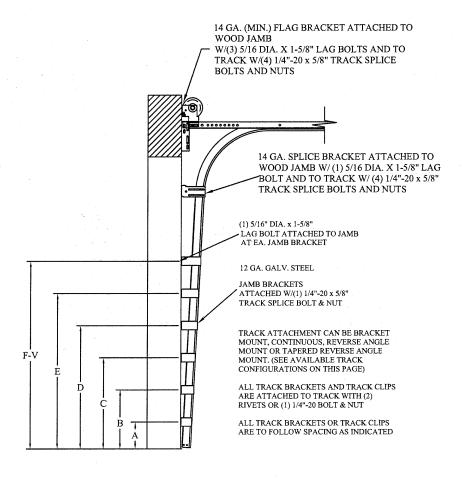


TABLE 1

Sec	tion	Cente	r Stile Lo	cations	Max Design Loads				
Wie		(Mea	sured fro	m Left	Allowed				
(f		1st	2nd	3rd	Positive	Negitive			
1 6	L)	(in)	(in)	(in)	(PSF)	(PSF)			
6'	0"	36"	-	-	37.1	42.3			
6'	6' 2" 37" -			-	36.1	41.2			
6'	6' 4" 38" - 6' 6" 39" -		-	-	35.2	40.1			
6'			-	34.3	39.1				
6'	8"	40"	-	-	33.4	38.1			
6'	6' 10" 41"			_	32.6	37.2			
7'	0"	42"	-	-	31.8	36.3			
9'	9' 6" 37" 77" 9' 8" 38" 78"		76"	-	35.0	39.9			
9'			77"	-	34.5	39.4			
9'			-	34.1	38.9				
9'	10"	39"	79"	-	33.7	38.4			
10'	0"	40"	80"	-	33.3	37.9			
10'	2"	41"	81"		32.8	37.4			
10'	4"	4" 42" 82"		-	32.4	37.0			
10'	6"	43"	83"	-	32.0	36.5			
10'	8"	44"	84"	-	31.7	36.1			
13'	0"	36"	78"	120"	31.7	36.1			
13'	2"	37"	79"	121"	31.7	36.1			
13'	4"	38"	80"	122"	31.7	36.1			
13'	6"	39"	81"	123"	31.7	36.1			
13'	8"	40"	82"	124"	31.7	36.1			
13'	10"	41"	83"	125"	31.7	36.1			
14'	0"	42"	84"	126"	31.7	36.1			
14'	2"	43"	85"	127"	31.5	35.9			

^{*} CONTACT ENGINEERING FOR SIZES 7'2" THROUGH 9'2" & 10'10" THROUGH 12'10"

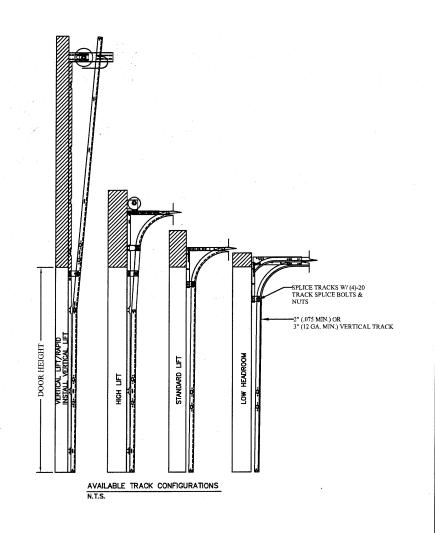


TRACK CONFIGURATION FOR UP TO 24' TALL DOORS
SEE TABLE 2



DC	OR	Ī .	TRACK ATTACHMENT														TYPICAL							
HEI	GHT	Α	В	С	D	Ε	· F	G	H	1	J	K	L	M	N	0	. P	Q	R	S	Т	U	V	SPLICE
7'	0"	10.0"	22.0"	34"	46"	58"																		76"
7'	6"	10.0"	22.0"	34"	46"	58"	70"					T				1 1				T				82"
8'	0"	10.0"	22.0"	34"	46"	58"	70"																	88"
8'	6"	10.0"	22.0"	34"	46"	58"	70"	82"	1.0															94"
9'	0"	10.0"	22.0"	34"	46"	58"	70"	82"																100"
9'	6"	10.0"	22.0"	34"	46"	58"	70"	82"	94"															106"
10'	0"	10.0"	22.0"	34"	46"	58"	70"	82"	94"															112"
11'	0"	10.0"	22.0"	34"	46"	58"	70"	82"	94"	106"										-				124"
12'	0"	10.0"	22.0"	34"	46"	58"	70"	82"	94"	106"	118"													136"
13'	0"	10.0"	22.0"	34"	46"	58"	70"	82"	94"	106"	118"	130"						-		1				148"
14'	0"	10.0"	22.0"	34"	46"	58"	70"	82"	94"	106"	118"	130"	142"											160"
15'	0"	10.0"	22.0"	34"	46"	58"	70"	82"	94"	106"	118"	130"	142"	154"						T	T			172"
16'	0"	10.0"	22.0"	34"	46"	58"	70"	82"	94"	106"	118"	130"	142"	154"	166"									184"
17'	0"	10.0"	22.0"	34"	46"	58"	70"	82"	94"	106"	118"	130"	142"	154"	166"	178"		7 1		1				196"
18'	0"	10.0"	22.0"	34"	46"	58"	70"	82"	94"	106"	118"	130"	142"	154"	166"	178"	190"							208"
19'	0"	10.0"	22.0"	34"	46"	58"	70"	82"	94"	106"	118"	130"	142"	154"	166"	178"	190"	202"			T	T		220"
20'	0"	10.0"	22.0"	34"	46"	58"	70"	82"	94"	106"	118"	130"	142"	154"	166"	178"	190"	202"	214"					232"
21	0"	10.0"	22.0"	34"	46"	58"	70"	82"	94"	106"	118"	130"	142"	154"	166"	178"	190"	202"	214"	226"				244"
22	0"	10.0"	22.0"	34"	46"	58"	70"	82"	94"	106"	118"	130"	142"	154"	166"	178"	190"	202"	214"	226"	238"			256"
23	0"	10.0"	22.0"	34"	46"	58"	70"	82"	94"	106"	118"	130"	142"	154"	166"	178"	190"	202"	214"	226"	238"	250"		268"
24	0"	10,0"	22.0"	34"	46"	58"	70"	82"	94"	106"	118"	130"	142"	154"	166"	178"	190"	202"	214"	226"	238"	250"	262"	280"

ALL TRACK ATTACHMENTS +/- 2" ALLOWED USING SYP OR SPF NO.2 OR BETTER ONLY



REV	DESCRIPTION (of Revis	IONS	DATE	BY
14'2 DESIG +3 -3 TES +4'	X SIZE 2 x 24' GN LOADS 1.5 PSF 5.9 PSF T LOADS 7.3 PSF 3.9 PSF	Thomas L. Shelmerdine, PE (TX PE #85829) Structural Solutions, PA (TX Firm #F-004063)	TX		5921-G W. Friendly Ave., Greensboro, NC 27410
			Amari		
	MOD	EL	1000 AMA	RR	2432
SIZE	DRAWN BY	RLR	DATE 6/6/17		RAWING NUMBER
В	CHECKED BY	RLR	DATE 6/6/17	IBC-	-1014–150–15
165 CA	RRIAGE COL		EMATIC NSTON-SALEM, N.C. 2716	OS SHEET	4 OF 4